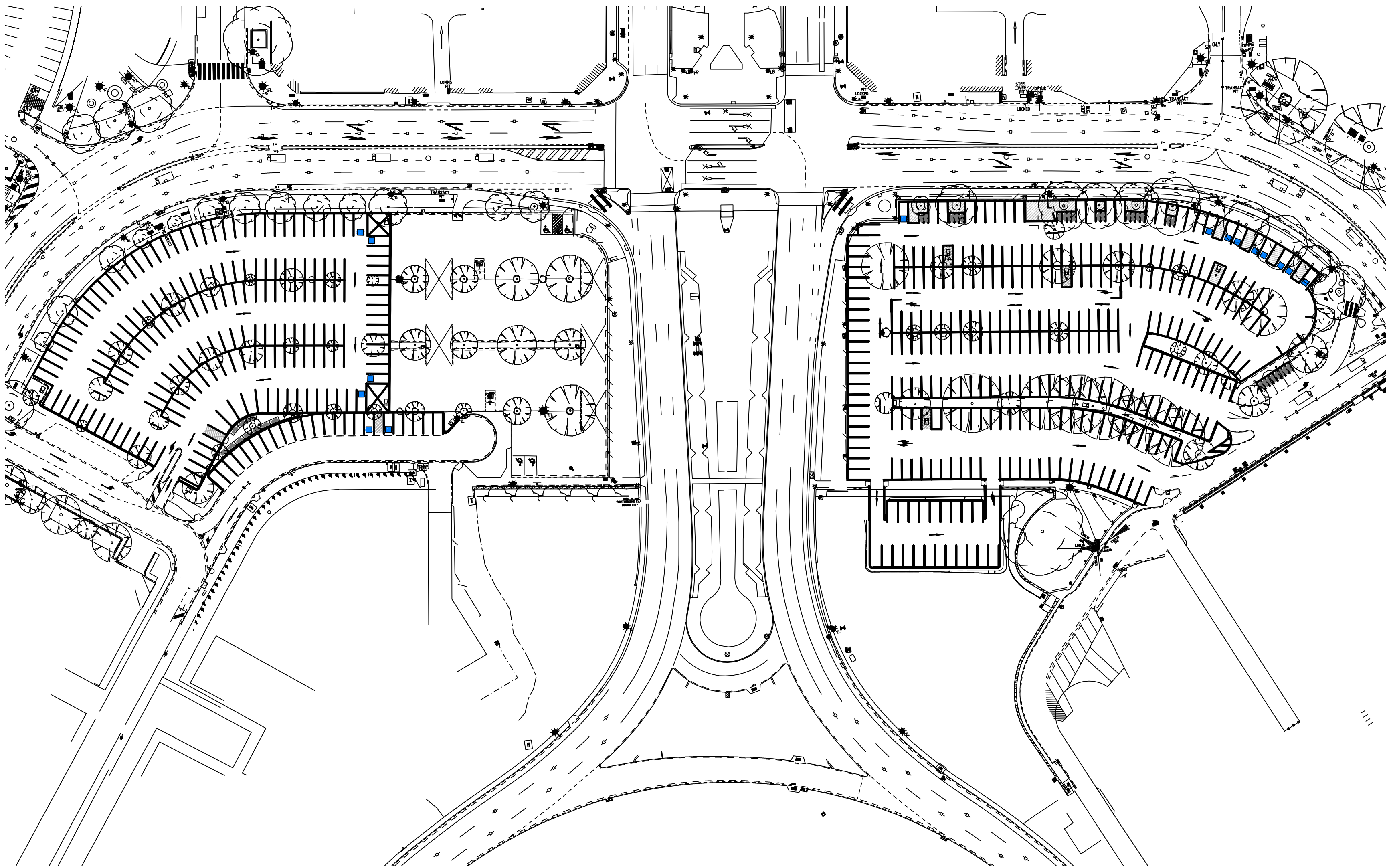


# CAPITAL METRO PROJECT

## TEMPORARY CAR PARK

### APPENDIX F - WORKS APPROVAL DRAWINGS



Issue	Date	By	Chkd	Appd

Issue	Date	By	Chkd	Appd

P02	19/01/16	NB	RA	RA
Issued for Works Approval				
P01	15/01/15	NB	RA	RA
Issued for Works Approval				
Issue	Date	By	Chkd	Appd

Designer

Client

Job Title  
Capital Metro  
Temporary Car Parks  
Works Approval

Temporary Car Parks  
Cover Sheet

Scale at A1 NOT TO SCALE	
Discipline General	
Job No 235067	Drawing Status Works Approval
Drawing No CLR-CGN-DRG-4001	Issue P02

A1	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P
1	<div><div>GENERAL</div><div><div>CLR-CGN-DRG-4001</div><div>CLR-CGN-DRG-4002</div></div><div>TEMPORARY CAR PARKS</div><div>TEMPORARY CAR PARKS</div><div>COVER SHEET</div><div>DRAWING INDEX</div></div>															
	<div><div><div>CLR-CGN-DRG-4005</div><div>CLR-CGN-DRG-4006</div><div>CLR-CGN-DRG-4007</div></div><div>TEMPORARY CAR PARKS</div><div>TEMPORARY CAR PARKS</div><div>TEMPORARY CAR PARKS</div><div>GENERAL LEGEND</div><div>NOTES</div><div>NOTES</div><div>SHEET 1 OF 2</div><div>SHEET 2 OF 2</div></div>															
2	<div><div><div>CLR-CGN-DRG-4008</div></div><div>TEMPORARY CAR PARKS</div><div>LOCALITY PLAN</div></div>															
	<div><div>SITE PLANS</div><div><div>CLR-CSP-DRG-4010</div><div>CLR-CSP-DRG-4011</div><div>CLR-CSP-DRG-4012</div></div><div>TEMPORARY CAR PARKS</div><div>TEMPORARY CAR PARKS</div><div>TEMPORARY CAR PARKS</div><div>DETAILED SITE PLAN</div><div>DETAILED SITE PLAN</div><div>DETAILED SITE PLAN</div><div>MAGISTRATES COURT CAR PARK</div><div>THEATRE CAR PARK</div><div>THEATRE CAR PARK</div><div>SHEET 1 OF 2</div><div>SHEET 2 OF 2</div></div>															
3	<div><div>CIVIL &amp; EXCAVATION</div><div><div>CLR-CCW-DRG-4040</div><div>CLR-CCW-DRG-4041</div><div>CLR-CCW-DRG-4042</div></div><div>TEMPORARY CAR PARKS</div><div>TEMPORARY CAR PARKS</div><div>TEMPORARY CAR PARKS</div><div>CIVIL &amp; EXCAVATION</div><div>CIVIL &amp; EXCAVATION</div><div>CIVIL &amp; EXCAVATION</div><div>MAGISTRATES COURT CAR PARK</div><div>THEATRE CAR PARK</div><div>THEATRE CAR PAR</div><div>PAVEMENT AND KERBS</div><div>PAVEMENT AND KERBS</div><div>PAVEMENT AND KERBS</div><div>SHEET 1 OF 2</div><div>SHEET 2 OF 2</div></div>															
	<div><div><div>CLR-CCW-DRG-4043</div><div>CLR-CCW-DRG-4044</div><div>CLR-CCW-DRG-4051</div></div><div>TEMPORARY CAR PARKS</div><div>TEMPORARY CAR PARKS</div><div>TEMPORARY CAR PARKS</div><div>CIVIL &amp; EXCAVATION</div><div>CIVIL &amp; EXCAVATION</div><div>CIVIL &amp; EXCAVATION</div><div>MAGISTRATES COURT CAR PARK</div><div>THEATRE CAR PARK</div><div>THEATRE CAR PARK</div><div>DRAINAGE AND UTILITIES</div><div>DRAINAGE AND UTILITIES</div><div>DRAINAGE AND UTILITIES</div><div>SHEET 1 OF 2</div><div>SHEET 2 OF 2</div></div>															
4	<div><div><div>ST-0014</div></div><div>STANDARD DRAWING</div><div>1050 ND MANHOLES</div></div>															
	<div><div><div>CLR-CCW-DRG-4049</div><div>CLR-CCW-DRG-4050</div></div><div>TEMPORARY CAR PARKS</div><div>TEMPORARY CAR PARKS</div><div>CIVIL &amp; EXCAVATION</div><div>CIVIL &amp; EXCAVATION</div><div>RAMP STRUCTURE DETAILS</div><div>RAMP STRUCTURE DETAILS</div><div>SHEET 1 OF 2</div><div>SHEET 2 OF 2</div></div>															
	<div><div>EROSION &amp; SEDIMENT CONTROL</div><div><div>CLR-CCW-DRG-4045</div><div>CLR-CCW-DRG-4046</div><div>CLR-CCW-DRG-4047</div><div>CLR-CCW-DRG-4048</div></div><div>TEMPORARY CAR PARKS</div><div>TEMPORARY CAR PARKS</div><div>TEMPORARY CAR PARKS</div><div>TEMPORARY CAR PARKS</div><div>EROSION &amp; SEDIMENT CONTROL</div><div>EROSION &amp; SEDIMENT CONTROL</div><div>EROSION &amp; SEDIMENT CONTROL</div><div>EROSION &amp; SEDIMENT CONTROL</div><div>MAGISTRATES COURT CAR PARK</div><div>THEATRE CAR PARK</div><div>THEATRE CAR PARK</div><div>TYPICAL DETAILS</div><div>SHEET 1 OF 2</div><div>SHEET 2 OF 2</div></div>															
	<div><div>LANDSCAPE</div><div><div>CLR-LLU-DRG-4031</div><div>CLR-LLU-DRG-4032</div><div>CLR-LLU-DRG-4033</div></div><div>TEMPORARY CAR PARKS</div><div>TEMPORARY CAR PARKS</div><div>TEMPORARY CAR PARKS</div><div>LANDSCAPING</div><div>LANDSCAPING</div><div>LANDSCAPING</div><div>MAGISTRATES COURT CAR PARK</div><div>THEATRE CAR PARK</div><div>THEATRE CAR PARK</div><div>SHEET 1 OF 2</div><div>SHEET 2 OF 2</div></div>															
	<div><div><div>CLR-LLU-DRG-4050</div><div>CLR-LLU-DRG-4051</div></div><div>TEMPORARY CAR PARKS</div><div>TEMPORARY CAR PARKS</div><div>TREE SURVEY PLAN</div><div>TREE SURVEY PLAN</div><div>MAGISTRATES COURT CAR PARK</div><div>THEATRE CAR PARK</div></div>															
7																
8																
9																
10																

Issue	Date	By	Chkd	Appd

Issue	Date	By	Chkd	Appd

P02	19/01/16	NB	RA	RA
Issued for Works Approval				
P01	15/01/15	NB	RA	RA
Issued for Works Approval				
Issue	Date	By	Chkd	Appd

Designer

ARUPHASSELL

Client

CapitalMetroACT Government

Job Title

Capital Metro  
Temporary Car Parks  
Works Approval

Temporary Car Parks  
Drawing Index

Scale at A1

NOT TO SCALE

Discipline

General

Job No

235067

Drawing Status

Works Approval

Drawing No


CLR-CGN-DRG-4002

Issue


P02




1 SITE PLANS – ROADS AND TRAFFIC




NCA/EPD BOUNDARY




EXISTING PARKING LAYOUT




PROPOSED PARKING LAYOUT




NO PARKING




PARKING FOR PEOPLE WITH DISABILITIES




MOTORCYCLE PARKING




PROPOSED TICKET VENDING MACHINE




TRAFFIC DIRECTION ARROW




PROPERTY BOUNDARY




SURFACE FEATURES




EXISTING TICKET VENDING MACHINE




EXISTING LIGHT POLE



EXISTING BOLLARD




EXISTING SURFACE WATER FLOW DIRECTION




EXISTING RUBBISH BIN

2 SITE PLANS – EARTHWORKS



NEW PAVEMENT (CAR PARK)




RESURFACING




MEDIAN / ISLAND (IF REQUIRED)


3 EROSION AND SEDIMENT CONTROL




DIRECTION OF SURFACE FLOW




GRAVEL KERB INLET SEDIMENT TRAP




SEDIMENT FENCE



SANDBAG SEDIMENT TRAP




TEMPORARY CONSTRUCTION ACCESS / EGRESS




SEDIMENT TRAP


4 DRAINAGE




TAMS TYPE R KERB INLET PIT



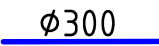
TAMS TYPE QS KERB INLET PIT




PRECAST CONCRETE MANHOLE



EXISTING MANHOLE




PROPOSED PIPE & SIZE




DRAINAGE FLOW DIRECTION


5 UTILITIES




ELECTRICITY – HIGH VOLTAGE UNDERGROUND




ELECTRICITY – LOW VOLTAGE UNDERGROUND




ELECTRICITY – STREET LIGHTING




GAS – HIGH PRESSURE




SEWER




WATER




STORMWATER DRAINAGE




TELSTRA




OPTUS




ICON COMMUNICATIONS



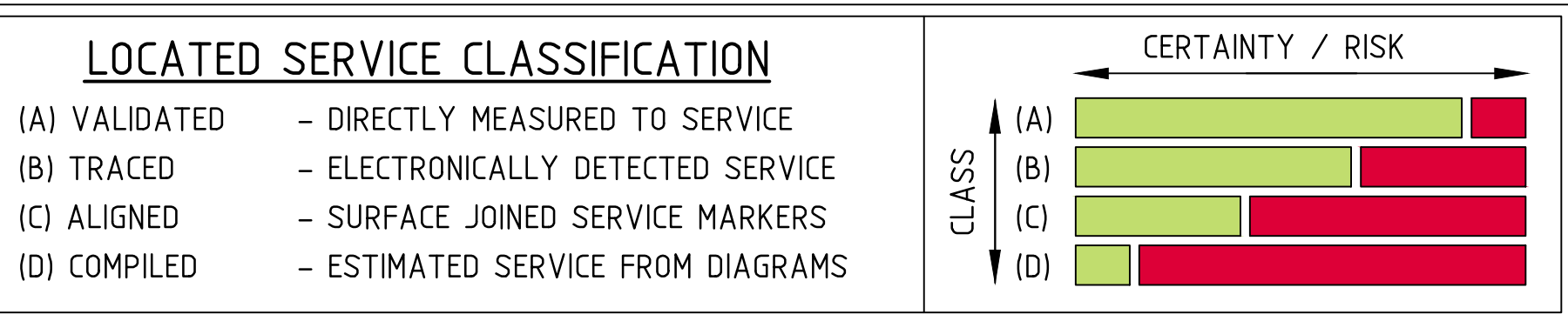
NBN Co.



RELOCATED UTILITY (HIGHLIGHTED RED)



DENOTES SERVICE LOCATION CLASSIFICATION (REFER TABLE)

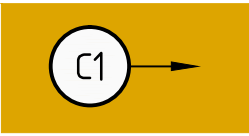


6 WARNING

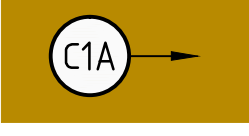
- CLASSIFICATION CHART IS PROVIDED TO INDICATE CERTAINTY OF IDENTIFIED SERVICE SHOWN OR LOCATED.
- REFER TO "AS5488-CLASSIFICATION OF SUBSURFACE UTILITY INFORMATION" 2013
- SERVICES SHOWN CLASS "B", "C" OR "D" MAY BE INACCURATE IN BOTH VERTICAL AND HORIZONTAL POSITION.
- MULTIPLE SERVICES MAY STILL EXIST IN SINGLE TRENCH, THEREFORE, SERVICES SHOWN MAY NOT REFLECT OTHER SERVICES ABOVE OR BELOW.
- SINGLE LINES SHOWN MAY NOT REPRESENT MULTIPLE CONDUITS, PIPES OR CABLES OF SERVICES LOCATED.
- UNKNOWN SERVICES MAY STILL EXIST THAT COULD NOT BE ELECTRONICALLY DETECTED. PLEASE CONSULT SERVICE PROVIDERS DIAGRAMS OR CALL "DIAL-BEFORE-YOU-DIG" SERVICE PROVIDER TO IDENTIFY DUTY OF CARE OBLIGATIONS PRIOR TO ANY EXCAVATION/CONSTRUCTION.

7 ARCHITECTURAL / URBAN DESIGN

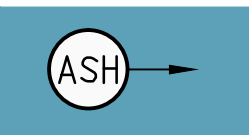
8 CONCRETE



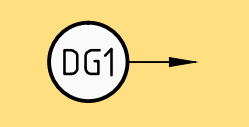
IN-SITU CONCRETE  
BROOM FINISHED  
PEDESTRIAN



IN-SITU CONCRETE  
BROOM FINISHED  
TRAFFICABLE



ASPHALTIC CONCRETE  
TRAFFICABLE



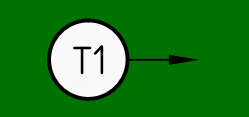
CEMENT STABILISED DECOMPOSED GRANITE  
TRAFFICABLE

9 KERBS




CONCRETE KERB  
REFER ROAD & CIVIL WORKS DRAWINGS

10 PLANTING




TURF  
ROLLED

11 LIGHTING



KIM LIGHTING "THE ARCHETYPE" FULL CUT-OFF LUMINAIRE SINGLE REACH.  
POWDER COATED FINISH, CHARCOAL COLOUR LIGHTING COLUMNS. (FINAL COLOUR SPECIFICATION TO BE CONFIRMED BY TAMS AND NCA)




KIM LIGHTING "THE ARCHETYPE" FULL CUT-OFF LUMINAIRE DOUBLE REACH  
POWDER COATED FINISH, CHARCOAL COLOUR LIGHTING COLUMNS. (FINAL COLOUR SPECIFICATION TO BE CONFIRMED BY TAMS AND NCA)

12 ACCESS

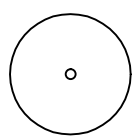


ROUTE TO ACCESSIBLE CAR PARKING SPACE

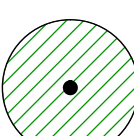
13 TREE IMPACT PLAN LEGEND



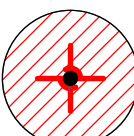
INDICATIVE TREE PROTECTION ZONE (BLUE) AS PER ARBORIST REPORT



EXISTING TREE NOT SURVEYED INDICATIVE LOCATION ONLY



TREE TO BE RETAINED



TREE TO BE REMOVED



A1	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P
1																
2																
3																
4																
5																
6																
7																
8																
9																
10																

GENERAL

- ALL DIMENSIONS ARE IN MILLIMETRES AND ALL LEVELS IN mAHD UNO.
- NO DIMENSIONS ARE TO BE OBTAINED BY SCALING FROM DRAWINGS.
- THESE DRAWINGS SHALL BE READ IN CONJUNCTION WITH THE ACT DESIGN STANDARDS FOR URBAN INFRASTRUCTURE STANDARD DRAWINGS AND ALL OTHER DRAWINGS AND SPECIFICATIONS.
- ALL WORKS TO BE IN ACCORDANCE WITH ACT DESIGN STANDARDS FOR URBAN INFRASTRUCTURE.
- ALL MATERIALS AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH AUSTRALIAN STANDARDS AND CODES OF PRACTICE EXCEPT WHERE VARIED BY THE DRAWINGS. THE APPLICABLE STANDARDS SHALL BE THE REFERENCED STANDARDS CURRENT AT DATE OF DRAWING ISSUE.

EROSION AND SEDIMENT CONTROL

- THE EROSION AND SEDIMENT CONTROL PLAN ADDRESSES THE MANAGEMENT OF ON SITE STORMWATER RUNOFF DURING CONSTRUCTION. IT DOES NOT ADDRESS GROUND WATER MANAGEMENT / DEWATERING REQUIREMENTS. IT IS TO BE READ IN CONJUNCTION WITH ALL OTHER GEOTECHNICAL, ENVIRONMENTAL AND STRUCTURAL DOCUMENTATION. THIS PLAN IS CONCEPT ONLY. SITE CONDITIONS AND PHASING OF WORKS ARE LIKELY TO INFLUENCE CONTROL MEASURES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR AMENDING THE SCHEME TO SUIT CONDITIONS AT THE TIME OF WORKS AND CONSTRUCTION PROGRAM.
- THE CONTRACTOR IS TO LIAISE AS NECESSARY WITH THE EPA AND OBTAIN AGREEMENT TO THE ADEQUACY OF THE PROTECTION WORKS. ALL WORKS SHALL COMPLY WITH THE REQUIREMENTS OF THE APPROVED CONSTRUCTION ENVIRONMENTAL MANAGEMENT PLAN (EMP) AND EPA APPROVALS ARE TO BE OBTAINED BEFORE WORKS COMMENCE.
- THE CONTRACTOR IS TO CONSTRUCT AND MAINTAIN PROTECTION WORKS IN ACCORDANCE WITH THE 'ENVIRONMENT PROTECTION GUIDELINES - CONSTRUCTION AND LAND DEVELOPMENT IN THE ACT' (2007) AND THE NSW BLUE BOOK AS APPROPRIATE.
- THE CONTRACTOR IS TO INFORM ALL BUILDERS AND SUB CONTRACTORS OF THEIR RESPONSIBILITIES IN MINIMISING THE POTENTIAL FOR SOIL EROSION AND POLLUTION TO ROADWAYS AND WATERWAYS.
- THE CONTRACTOR IS TO IMPLEMENT AN APPROPRIATE ENVIRONMENTAL MANAGEMENT PLAN INCLUDING SPILL/ POLLUTION CONTAINMENT AND TREATMENT PROCEDURES. THE CONTRACTOR IS TO ENSURE THAT ANY SPILL/POLLUTION COLLECTED IS IMMEDIATELY TREATED.
- WATER SHALL BE PREVENTED FROM ENTERING THE PERMANENT DRAINAGE SYSTEM UNLESS THE CATCHMENT AREA HAS BEEN PERMANENTLY LANDSCAPED AND/OR IT HAS BEEN TESTED FOR COMPLIANCE IN ACCORDANCE WITH NOTE 16.
- WORKS SHALL BE UNDERTAKEN IN THE FOLLOWING SEQUENCE;
  - INSTALL SOIL AND WATER MANAGEMENT WORKS
  - CARRY OUT EARTHWORKS
  - PROTECT WORKS
  - UNDERTAKE REMAINING SITE WORKS IN ACCORDANCE WITH ENGINEERING PLANS AND SPECIFICATION
  - REMOVE SOIL AND WATER MANAGEMENT WORKS NOT REQUIRED FOR OTHER STAGES OF CONSTRUCTION ONCE UPSTREAM SURFACES ARE STABILISED TO THE SATISFACTION OF THE SUPERINTENDENT.
- SAND, CEMENT AND OTHER EASILY WIND-BORNE MATERIAL STOCKPILES ARE TO BE COVERED AND DUST CONTROL MEASURES IMPLEMENTED CONTINUOUSLY DURING CONSTRUCTION WORKS TO THE SATISFACTION OF THE SUPERINTENDENT.
- STOCKPILE LOCATIONS TO BE DEPENDENT ON THE LOAD OUT LOCATION AND THE POINT OF EXCAVATION. STOCKPILE LOCATIONS TO BE MARKED ON THE SITE PLAN AT THE SITE OFFICE AS THE PROJECT PROGRESSES.
- SHOULD ANY MATERIAL BE WASHED FROM EQUIPMENT, SUCH AS CONCRETE SLURRIES FROM CONCRETE TRUCKS, A WASHING/CLEANING AREA WITH APPROPRIATE SEDIMENT CONTROL MEASURES IS TO BE

SET UP ON A FLAT AREA OF THE SITE.

- THE CONTRACTOR SHALL MAINTAIN A LOG BOOK. THE LOG BOOK SHALL BE MAINTAINED ON A WEEKLY BASIS AND BE MADE AVAILABLE TO ANY AUTHORISED PERSON UPON REQUEST. THE ORIGINAL LOG BOOK SHALL BE ISSUED TO THE PROJECT MANAGER AT THE COMPLETION OF THE WORKS.
  - CONTROL MEASURES AFFECTED BY WORKS ARE TO BE RE-ESTABLISHED PRIOR TO THE COMPLETION OF EACH DAYS WORK.
  - ALL CONTROL MEASURES ARE TO BE CLEANED AND MAINTAINED AT LEAST WEEKLY OR AFTER EVERY RAINFALL EVENT.
  - FOLLOWING THE COMPLETION AND RESTORATION OF SITE, THE CONTRACTOR IS TO REMOVE ALL CONTROL MEASURES.
  - PERMANENT DRAINAGE STRUCTURES INCLUDING PIPES AND PITS ARE TO BE HANDED OVER IN A CLEAN CONDITION AT THE COMPLETION OF THE CONTRACT MAINTENANCE PERIOD.
  - PRIOR TO DISCHARGING COLLECTED WATER TO STORMWATER DRAINAGE, IT IS TO BE TESTED TO ENSURE COMPLIANCE WITH THE FOLLOWING WATER QUALITY REQUIREMENTS:
    - TSS < 50mg/L.
    - OILS AND GREASES <10MG/L. A VISUAL ASSESSMENT TO DETERMINE PRESENCE OF OILS & GREASES IS SUFFICIENT.
    - 6.5 < pH < 8.5
- SHOULD TESTING GIVE RESULTS THAT DO NOT COMPLY WITH THE ABOVE, TREATMENT MEASURES (SUCH AS THE APPLICATION OF A pH NEUTRAL FLOCCULENT) AND SUBSEQUENT RETESTING IS REQUIRED. DOCUMENTARY RESULTS OF WATER QUALITY TESTING PRIOR TO DEWATERING ARE TO BE KEPT. A FILE IS TO BE KEPT ONSITE OF ALL WATER TESTING/DEWATERING EVENTS. FOLLOWING DEWATERING THE SEDIMENT BASIN IS TO BE CLEARED OF SEDIMENT.
- ALL STORMWATER PITS TO BE COVERED OR DROP INLET SEDIMENT TRAPS SHALL BE PROVIDED. KERB INLET TRAPS ARE TO BE INSTALLED AFTER COMPLETION OF PAVING.
  - ALL SERVICE TRENCHES MUST BE FILLED IN AND COMPACTED IMMEDIATELY AFTER SERVICES HAVE BEEN LAID.
  - ROADS AND FOOTPATHS AFFECTED BY THE WORKS MUST BE SWEEP CLEAN DAILY. SOILS MUST BE RETAINED BEHIND CONTROL DEVICES.
  - CONTRACTOR MUST ENSURE THAT ALL VEHICLES LEAVING SITE ARE HOSED DOWN (OR SIMILAR) TO REMOVE SEDIMENT.
  - SITE ENTRY/ EXIT TO BE INSTALLED WITH RUMBLE GRIDS.

EARTHWORKS

- EXCAVATED SURFACES ARE TO BE CHECKED FOR SOFT OR COMPRESSIBLE ZONES. SUCH ZONES ARE TO BE OVER EXCAVATED TO A MINIMUM DEPTH OF 300mm AND REPLACED WITH COMPACTED SELECTED MATERIAL.
- THE CONTRACTOR SHALL ENSURE THAT THE SUBGRADE IS PROPERLY PREPARED AND COMPACTED.
- UNO, LOCAL BATTERS AT MIN. 1H:1V SHALL BE PROVIDED AT STEPS IN EARTHWORKS LEVEL.
- THE CONTRACTOR IS RESPONSIBLE FOR ALL TEMPORARY WORKS INCLUDING SHORING AND BATTERS. ALL TEMPORARY WORKS ARE TO BE DESIGNED IN ACCORDANCE WITH AUSTRALIAN STANDARDS AND CODES OF PRACTICE BY A SUITABLY QUALIFIED ENGINEER.
- ALL EARTHWORKS AND DISPOSAL OF MATERIALS IS TO BE UNDERTAKEN IN ACCORDANCE WITH THE REQUIREMENTS OF THE SUPERINTENDENT.
- ANY EXCAVATED MATERIAL TO BE REMOVED FROM THE SITE IS TO BE ASSESSED, CLASSIFIED, TRANSPORTED AND DISPOSED OF IN ACCORDANCE WITH THE DEPARTMENT OF ENVIRONMENT AND CLIMATE CHANGE'S (DECC) 'WASTE CLASSIFICATION GUIDELINES PART 1: CLASSIFYING WASTE'
- ANY FILL MATERIAL IMPORTED INTO THE SITE IS TO BE VIRGIN EXCAVATED NATURAL MATERIAL OR MATERIAL SUBJECT TO A RESOURCE RECOVERY EXEMPTION THAT IS PERMITTED TO BE USED AS FILL MATERIAL, IN ACCORDANCE WITH THE PROVISIONS OF THE PROTECTION OF THE ENVIRONMENT OPERATIONS ACT 1997 AND THE PROTECTION OF THE ENVIRONMENT (WASTE) REGULATION 2005.

- ANY FILL MATERIAL SUBJECT TO A RESOURCE RECOVERY EXEMPTION RECEIVED AT THE SITE MUST BE ACCOMPANIED BY DOCUMENTATION DEMONSTRATING THAT MATERIAL'S COMPLIANCE WITH THE CONDITIONS OF THE EXEMPTION, AND THIS DOCUMENTATION MUST BE PROVIDED TO COUNCIL OFFICERS OR THE PRINCIPAL CERTIFYING AUTHORITY ON REQUEST.
- TOPSOIL OR SOIL CONTAINING A HIGH PROPORTION OF ROOT MATTER MUST BE STRIPPED FROM THE SITE AND DISPOSED OF.

PAVEMENTS, KERBS, FOOTWAYS AND ANCILLARY ITEMS

- PAVEMENT DESIGNS HAVE BEEN BASED UPON A DESIGN CBR OF 3%. THE CONTRACTOR IS TO CONFIRM THE IN-SITU CBR AND NOTIFY THE SUPERINTENDENT OF ANY DISCREPANCY.
- PAVEMENTS ARE TO BE NEW CONSTRUCTION OR MILLED AND RESURFACED AS INDICATED ON THE PAVEMENT LAYOUT DRAWINGS. PAVEMENT BUILDUP IS TO BE AS PER THE VALUES SHOWN BELOW:

NEW PAVEMENTS:

DESIGN TRAFFIC 1X10<sup>3</sup> ESAS

  - 35MM AC10-MD SURFACING
  - AMC0 PRIME @ 1 LITRE/M<sup>2</sup>
  - 100MM NGB20-2D, 20MM NOMINAL SIZED NATURAL GRAVEL BASE
  - 200MM NGS20, 20MM NOMINAL SIZED NATURAL GRAVEL SUB-BASE
  - SUBGRADE CBR 3% MIN.

RESURFACING:

  - MILL AND SCARIFY NOMINAL 35MM EXISTING ASPHALT SURFACING
  - 35MM AC10-MD SURFACING
  - CORRECTOR COURSE (PROVISIONAL)
  - TACK COAT CRS170/60 @ 0.2 LITRE/M<sup>2</sup>

MEDIAN/ISLAND (IF REQUIRED):

  - 100MM HAND PLACED CONCRETE, 25 MPA WITH SL62 MESH
- PAVEMENT MATERIALS AND CONSTRUCTION ARE TO BE IN ACCORDANCE WITH THE RELEVANT ACT URBAN SERVICES STANDARD SPECIFICATION FOR INFRASTRUCTURE WORKS.
- THE SUPERINTENDENT MAY REQUEST A PROOF ROLL OF THE SUBGRADE TO DETERMINE THE ACCURACY OF THE COMPACTIVE EFFORT.
- THE PRIME IS TO BE ALLOWED TO CURE AND TURN FROM BROWN TO BLACK PRIOR TO BEING OVERLAID WITH ASPHALT. NO TRAFFIC IS PERMITTED ON THE PRIME.
- THE FINISHED ASPHALT SURFACE SHALL NOT DEVIATE MORE THAN 6mm ON A 3m STRAIGHT EDGE IN ANY DIRECTION.

DRAINAGE

- THE LOCATION OF EXISTING DRAINAGE INFRASTRUCTURE SHOWN ON THE DRAWINGS IS BASED UPON AVAILABLE INFORMATION AND IS INDICATIVE ONLY. NO RESPONSIBILITY IS TAKEN FOR ITS ACCURACY OR COMPLETENESS. ANY DISCREPANCY IN THIS INFORMATION MAY AFFECT THE DESIGN.
- DETAILS OF ALL DRAINAGE INFRASTRUCTURE ARE TO BE CONFIRMED PRIOR TO WORKS BEGINNING. IT IS ASSUMED THAT NO UPGRADING OF EXISTING DRAINAGE NETWORKS IS REQUIRED.
- UNMAPPED OR UNKNOWN EXISTING DRAINAGE INFRASTRUCTURE MAY BE PRESENT WITHIN THE PROJECT EXTENTS, WHICH MAY IMPACT ON THE VIABILITY OF THE PROPOSED WORKS SHOWN. THESE ITEMS MAY REQUIRE ADDITIONAL PROTECTION / TREATMENT.
- DRAINAGE COORDINATION WITH EXISTING AND PROPOSED UTILITIES, LANDSCAPE TREATMENTS OR STREET FURNITURE IS BASED ON AVAILABLE SURVEY INFORMATION AND DBYD SEARCH.
- THE CONDITION OF EXISTING DOWNSTREAM DRAINAGE INFRASTRUCTURE IS CURRENTLY UNKNOWN. IT HAS BEEN ASSUMED THAT EXISTING DRAINAGE INFRASTRUCTURE IS IN A GOOD CONDITION AND WILL NOT REQUIRE IMPROVEMENT PRIOR TO UNDERTAKING THE WORKS. THE CONTRACTOR IS TO CONFIRM THIS USING VISUAL

STRUCTURAL INSPECTION OF ACCESS CHAMBERS / GULLY PITS, AND CCTV SURVEY OF PIPEWORK.

- WHERE EXISTING PERVIOUS CAR PARKING AREA IS SHOWN TO BECOME IMPERVIOUS, ON-SITE DETENTION HAS BEEN CONSIDERED TO LIMIT STORMWATER FLOWS TO EXISTING. AN INDICATION OF POTENTIAL REQUIREMENTS HAS BEEN PROVIDED ON THE DRAINAGE LAYOUT DRAWINGS.
- FOR ALL AREAS WHERE THE URBAN DESIGN TREATMENT INDICATES NEW SURFACING, ALLOWANCE SHOULD BE MADE FOR NEW ACCESS COVERS WITH INFILL OF THE APPROPRIATE SURFACING MATERIAL.
- ALLOWANCE SHOULD BE MADE FOR ADJUSTMENT TO THE LEVEL OF THE MAJORITY OF ACCESS COVERS WITHIN THE AFFECTED PAVEMENT AS A RESULT OF RE-SURFACING WORKS. THIS MAY REQUIRE STRUCTURAL CHANGES TO EXISTING PITS.
- IT IS ASSUMED THAT TREATMENT OF TOTAL SUSPENDED SOLIDS THROUGH APPROPRIATE WATER SENSITIVE URBAN DESIGN (WSUD) MEASURES, INCLUDING PROPRIETARY STORMWATER QUALITY IMPROVEMENT DEVICES (SQIDS), IS NOT REQUIRED. OPPORTUNITIES FOR WSUD SHALL BE CONSIDERED DURING DETAILED DESIGN.
- THE LOCATION AND LEVEL OF ALL SERVICES WHICH CROSS PROPOSED DRAIN LINES MUST BE CONFIRMED PRIOR TO CONSTRUCTION AND BE CHECKED FOR CONFLICT. ANY CONFLICTS ARE TO BE REPORTED TO THE SUPERINTENDENT AND ADVICE SOUGHT PRIOR TO COMMENCING WORKS.
- WHERE A CONNECTION IS TO BE MADE TO AN EXISTING DRAINAGE PIPE OR STRUCTURE, THE LEVEL OF THAT PIPE OR STRUCTURE MUST BE CONFIRMED PRIOR TO THE CONSTRUCTION OF THE NEW DRAIN LINE.
- ALL PIPES 300mm OR GREATER ARE TO BE SRCP, RRJ AND PIPE CLASS 4, UNO. PIPE CLASSES HAVE BEEN CHECKED FOR THE PERMANENT DESIGN CASE AND DO NOT ALLOW FOR CONSTRUCTION LOADING. THE CONTRACTOR IS TO CHECK TEMPORARY LOADING CONDITIONS ARE WITHIN THE SPECIFIED PIPE CLASS OR THE PIPE CLASS IS TO BE INCREASED AS NECESSARY.
- ALL PIPES LESS THAN 300mm TO BE uPVC, SEWER GRADE, SOLVENT WELDED UNO.
- DRAINAGE LINES ARE TO BE LAID AT A MINIMUM GRADE OF 0.5% OR AS SHOWN ON THE LONGITUDINAL SECTIONS.
- INSTALLATION, BEDDING AND BACKFILLING OF ALL DRAINAGE PIPES SHALL BE HS3 IN ACCORDANCE WITH AS3725. MARKER LAYERS SHALL BE INSTALLED IN ALL TRENCHES IN ACCORDANCE WITH THE CIVIL SPECIFICATION.
- PIPES SHALL BE CONNECTED TO PITS SUCH THAT THE PIPE IS CENTRED ON THE PIT FACE.
- SETOUT POINTS AND SURFACE LEVELS FOR PITS AND MANHOLES SHALL BE THE CENTRE OF THE STRUCTURES, UNO ON DETAIL PLANS.
- GRATE FRAME MUST PROVIDE FIRM SUPPORT TO ALL SIDES ENSURING GRATES ARE SECURE.
- ALL GRATES ARE TO BE CLASS D, BICYCLE TYRE PENETRATION RESISTANT IN ACCORDANCE WITH CLAUSE 3.3.6 OF AS 3996:2005 AND HEELSAFE, UNO.
- PIPE JOINT TO BE PROVIDED 600mm FROM EXTERNAL FACE OF STORMWATER PITS FOR PIPES ≤ 450mm. FOR PIPES > 450mm, PIPE JOINT IS TO BE PROVIDED 1200mm FROM FACE OF STORMWATER PITS.
- PROVIDE 100mm DIA. SLOTTED SUBSOIL PIPE CONNECTION TO STORMWATER PITS. THE PIPE IS TO BE 3.0m LONG ENCLOSED IN TUBULAR FILTER FABRIC AND LAID ADJACENT TO INLET PIPE/S.
- FLUSHING POINTS ARE TO BE PROVIDED ON ALL SUBSOIL DRAINAGE PIPES AT 30m CENTRES AND END OF PIPES.
- CONCRETE FOR DRAINAGE STRUCTURES TO BE N32.
- STORMWATER DRAINAGE CRITERIA:

MINOR STORM = 20y ARI

MAJOR STORM = 100y ARI
- ALL GRATED DRAINS, WATER QUALITY TREATMENT DEVICES, PITS AND PROPRIETARY PRODUCTS ARE TO BE INSTALLED TO THE MANUFACTURER'S SPECIFICATIONS. DESIGNATED PRODUCTS CAN BE SUBSTITUTED WITH EQUIVALENT PRODUCTS, BUT MUST FIRST BE SUBMITTED TO THE SUPERINTENDENT FOR APPROVAL.
- THE CONTRACTOR IS TO CARRY OUT A CCTV INSPECTION OF THE COMPLETED DRAINAGE WORKS THAT ARE TO REVERT TO TAMS AND

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Designer

ARUP  
HASSELL

Client

CapitalMetro  
ACT Government

Job Title

Capital Metro  
Temporary Car Parks  
Works Approval

Temporary Car Parks  
Notes  
Sheet 1 of 2

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Discipline General

Job No

235067

Drawing Status

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Drawing No

CLR-CGN-DRG-4006

Issue

P01



A1	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P
1	A DVD FORWARDED TO TAMS TO SUPPORT THE CERTIFICATION OF WORKS.															
2	<div><div>LINEMARKING AND SIGNPOSTING</div><div><div>1. LINE MARKING SHOWN IS INDICATIVE ONLY. PAVEMENT MARKING IS TO BE IN ACCORDANCE WITH RELEVANT SPECIFICATION AS DETERMINED DURING DETAILED DESIGN. MARKINGS SHALL BE IN (HOT APPLIED) NON-PROFIED THERMOPLASTIC PAINT.</div><div>2. ALL SIGNS SHALL COMPLY WITH AS 1906.1 FOR CLASS 1 MATERIAL.</div><div>3. EXACT LOCATION OF SIGNS IS TO BE CONFIRMED ON SITE WITH THE SUPERINTENDENT PRIOR TO INSTALLATION.</div><div>4. SIGNPOSTING IS TO BE IN ACCORDANCE WITH AS 1742.2, AS 1742.4, AS 1742.11, AS 1743, AS 1744 AND SIGN SUPPORTS ARE TO BE IN ACCORDANCE WITH AS 1742.2.</div></div></div>															
3	<div><div>UTILITIES</div><div><div>1. ALL EXISTING STRUCTURES, SERVICES AND UTILITIES ARE TO BE LOCATED BY THE CONTRACTOR PRIOR TO COMMENCEMENT OF WORKS. THE LOCATIONS OF EXISTING SERVICES SHOWN ON PLANS ARE INDICATIVE ONLY AND ARE NOT GUARANTEED TO BE COMPLETE OR CORRECT. THE RESPONSIBILITY FOR LOCATING, AVOIDANCE AND WHERE NECESSARY, TEMPORARY PROTECTION OF THESE EXISTING SERVICES IS THAT OF THE CONTRACTOR. ANY DAMAGE TO EXISTING STRUCTURES, SERVICES AND UTILITIES IS TO BE REPORTED TO THE SUPERINTENDENT IMMEDIATELY.</div><div>2. WHERE REQUIRED, THE CONTRACTOR SHALL ARRANGE SERVICE PROTECTION MEASURES WITH THE RELEVANT SERVICE AUTHORITY. THE CONTRACTOR IS RESPONSIBLE FOR CONFIRMING THE REQUIREMENTS OF SERVICE AUTHORITIES FOR WORKS IN THE VICINITY OF THEIR UTILITIES. WHERE REQUIRED, THE CONTRACTOR SHALL GAIN APPROVAL FROM THE SERVICE AUTHORITIES FOR THE PROPOSED WORKS.</div><div>3. THE CONTRACTOR SHALL ENSURE THAT AT ALL TIMES SERVICES NOT AFFECTED BY THE WORKS ARE NOT DISRUPTED WITHOUT PRIOR CONSENT FROM THE SERVICE AUTHORITIES OR ASSET OWNERS.</div><div>4. THE CONTRACTOR SHALL TAKE EVERY PRECAUTION NECESSARY TO PROTECT ALL EXISTING SERVICES FROM DAMAGE. ANY DAMAGE TO SERVICES IS TO BE REPORTED TO THE PRINCIPAL AND THE RELEVANT SERVICE AUTHORITIES IMMEDIATELY.</div><div>5. THE CONTRACTOR SHALL CONSTRUCT TEMPORARY SERVICES TO ENSURE EXISTING SUPPLY REMAINS IN OPERATION DURING WORKS TO THE SATISFACTION OF THE SERVICE AUTHORITIES. ONCE THE DIVERSION IS COMPLETED AND COMMISSIONED THE CONTRACTOR SHALL REMOVE ALL SUCH TEMPORARY SERVICES AND MAKE GOOD TO THE SATISFACTION OF THE SERVICE AUTHORITIES.</div><div>7. SYMBOLS REPRESENTING PHYSICAL STRUCTURES SUCH AS POWER POLES AND PITS ARE DIAGRAMMATIC ONLY AND DO NOT NECESSARILY REPRESENT THE ACTUAL SIZE AND EXTENT OF THESE FEATURES.</div><div>8. THE CONTRACTOR SHALL TAKE EVERY PRECAUTION NECESSARY TO PROTECT ALL EXISTING SERVICES FROM DAMAGE. ANY DAMAGE TO SERVICES IS TO BE REPORTED TO THE SUPERINTENDENT AND THE RELEVANT SERVICE AUTHORITIES IMMEDIATELY.</div><div>9. THE CONTRACTOR IS TO NOTE THE LOCATION OF ALL ACTEWAGL CABLES. ALL WORKS IN CLOSE PROXIMITY TO CABLES MUST COMPLY WITH ACTEWAGL GUIDANCE DOCUMENTS.</div><div>10. THE CONTRACTOR IS NOT TO USE VIBRATORY EQUIPMENT, EXCEPT FOR SMALL HAND HELD COMPACTION PLANT, OVER UNDERGROUND SERVICES. SERVICE PROVIDER REQUIREMENTS TO BE CONFIRMED BY CONTRACTOR PRIOR TO THE WORKS.</div><div>11. DBYD SEARCH HAS IDENTIFIED GOVERNMENT COMMUNICATIONS ASSETS IN THE AREA. THE LOCATION AND TREATMENT OF THESE ASSETS IS TO BE AGREED WITH THE ASSET OWNER DURING DETAILED DESIGN.</div><div>12. DETAILED DESIGN OF UTILITIES TO BE SUBJECT TO A SITE INVESTIGATION.</div><div>13. UTILITY RELOCATIONS ARE TO BE CONFINED TO THE WORKS AREA.</div></div></div>															
4	<div><div>URBAN DESIGN</div><div><div>GENERAL</div><div><div>1. ALL INFORMATION SHOWN WITHIN THE URBAN DESIGN DRAWING SET HAS BEEN PREPARED FOR THE PURPOSES OF WORKS APPROVAL AND DOES NOT REPRESENT A FULLY CO-ORDINATED SET OF INFORMATION. WHILST EVERY EFFORT HAS BEEN MADE TO ENSURE THAT CIVIL AND STRUCTURAL WORKS CO-ORDINATE WITH THE URBAN DESIGN INTENT, IT HAS NOT BEEN POSSIBLE TO ENSURE THAT ALL AREAS HAVE BEEN FULLY CAPTURED AND CO-ORDINATED.</div><div>2. PUBLIC DOMAIN MATERIAL TREATMENTS ARE SHOWN INDICATIVELY, AND ARE SUBJECT TO FURTHER CO-ORDINATION WITH NCA, TAMS AND KEY STAKEHOLDERS.</div></div></div></div> <div><div>SURVEY INFORMATION</div><div><div>3. THE PROPOSED SURFACE LEVELS, TREES TO BE REMOVED AND TREES TO BE TRANSPLANTED HAVE BEEN IDENTIFIED BASED ON SURVEY INFORMATION PROVIDED AT THE TIME OF DESIGN. FURTHER OR MORE DETAILED SURVEY AND UTILITIES INFORMATION MAY ALTER THE DESIGN.</div></div></div> <div><div>KERBS</div><div><div>4. GENERALLY, EXISTING KERBS HAVE BEEN AMENDED WHERE THERE IS A REDUCTION OR ADDITION VERTICALLY, HORIZONTALLY OR BOTH TO THE ROAD EDGE. UNLESS OTHERWISE NOTED ANY NEW KERBS WILL REQUIRE REINSTATEMENT OF THE ADJACENT PAVEMENT OR SOFT LANDSCAPING, MAKING GOOD AND MATCHING WITH EXISTING MATERIALS AND FINISHES.</div></div></div> <div><div>TREE IMPACT ASSESSMENT</div><div><div>5. THE TREE IMPACT ASSESSMENT DRAWINGS ARE REPRESENTATIVE OF INFORMATION GAINED FROM AN ARBORICULTURAL TREE ASSESSMENT BY NORCUE P/L. IMPACT ASSESSMENTS HAVE BEEN CONDUCTED IN ACCORDANCE WITH THE TREE PROTECTION ACT 2005 AND UTILISING A MULTI-CRITERIA ANALYSIS WITH REGARDS TO CONSTRUCTION IMPACTS, LOCAL CONDITIONS AND TREE HEALTH. TREES HAVE BEEN IDENTIFIED ACCORDING TO THEIR PROBABILITY FOR RETENTION, AND CLASSIFIED AS HIGH, MEDIUM AND LOW. THE DETERMINATION OF THE REMOVAL OF ANY TREES WILL BE SUBJECT TO FINAL ARBORIST'S ADVICE WHICH MAY REQUIRE DETAILED ROOT INVESTIGATION.</div></div></div>															
5	<div><div>POWER, LIGHTING AND COMMUNICATIONS</div><div><div>6. EXISTING WIFI NETWORK TO BE REINSTATED IN THE NEW CAR PARK</div><div>7. PROVIDE POWER AND COMMUNICATIONS SUPPLY TO EACH TICKET VENDING MACHINE (SUPPLIED BY THE TERRITORY). REQUIREMENTS TO BE DETERMINED DURING DETAILED DESIGN.</div><div>8. CAR PARK LIGHTING TO BE DESIGNED IN ACCORDANCE WITH AS/NZS 1158.3.1 P11(b) CATEGORY AND P12 CATEGORY DURING DETAILED DESIGN.</div><div>9. KIM LIGHTING "THE ARCHETYPE" LUMINAIRE MOUNTED ON 9m POLE TO BE 250w METAL HALIDE</div></div></div>															
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Notes  
Sheet 2 of 2

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Discipline

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Capital Metro  
Temporary Car Parks  
Works Approval

Temporary Car Parks  
Locality Plan

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