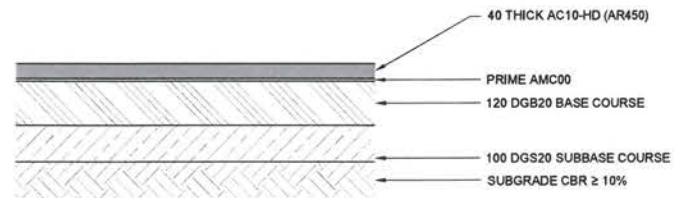
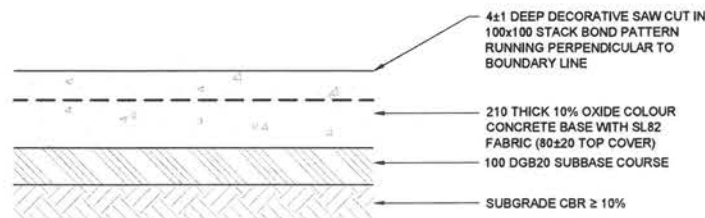
**PAVEMENT TYPE 1 (PT1)**

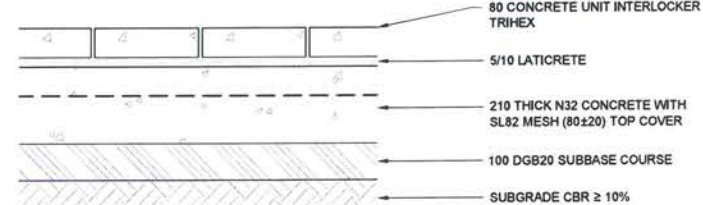
SCALE 1:10

DESIGN TRAFFIC =  $5 \times 10^5$  DESA  
TOTAL DEPTH = 280**PAVEMENT TYPE 2 (PT2)**

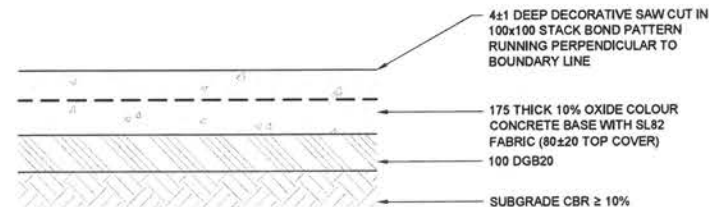
SCALE 1:10

DESIGN TRAFFIC =  $4 \times 10^5$  DESA  
TOTAL DEPTH = 260**PAVEMENT TYPE 3 (PT3)**

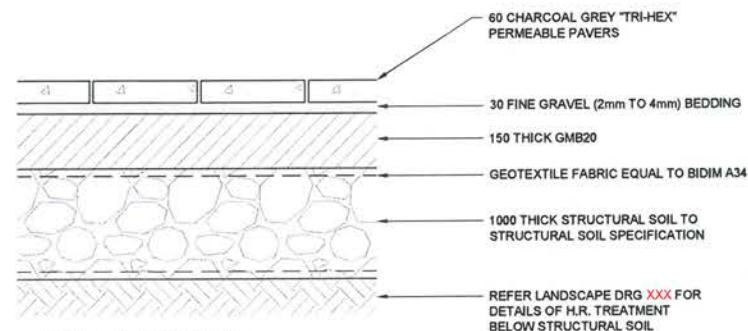
SCALE 1:10

DESIGN TRAFFIC =  $8 \times 10^5$  HVAG  
TOTAL DEPTH = 310**PAVEMENT TYPE 4 (PT4)**

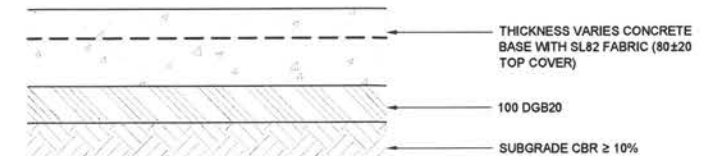
SCALE 1:10

DESIGN TRAFFIC =  $8 \times 10^5$  HVAG  
TOTAL DEPTH = 395**PAVEMENT TYPE 5 (PT5)**

SCALE 1:10

**PAVEMENT TYPE 6 (PT6)**

SCALE 1:10

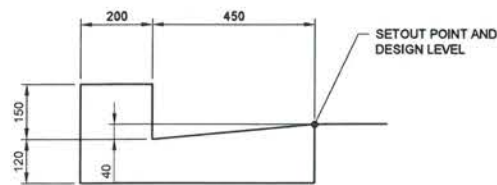
**PAVEMENT TYPE 7 (PT7)**

SCALE 1:10

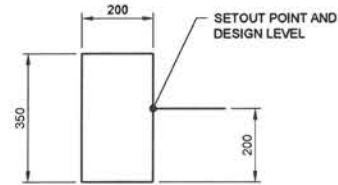
TOTAL DEPTH = 380

**NOTES**

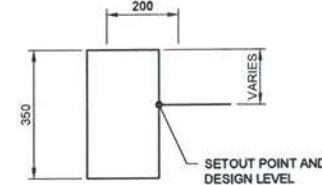
- ALL CORNERS AND RE-ENTERANT ANGLES SHALL BE ROUNDED TO 5mm RADIUS

**SPECIAL KERB AND GUTTER (SKG)**

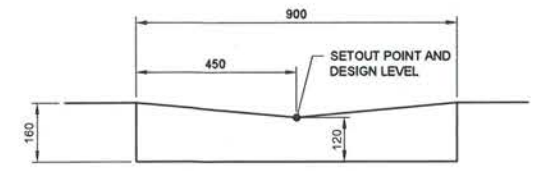
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**SPECIAL KERB ONLY (SKO) ADJACENT TO PT1 & PT2**

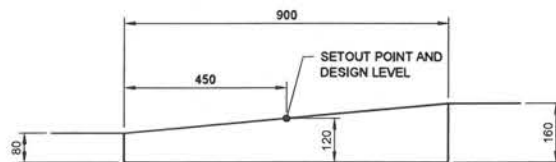
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**SPECIAL KERB ONLY (SKO) ADJACENT TO PT3, PT5A & PT5B**

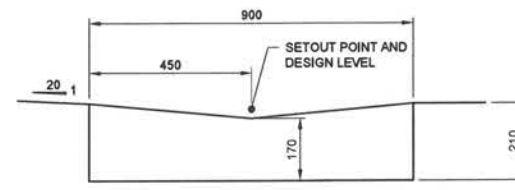
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**SPECIAL OPEN CONCRETE INVERT (SOCI)**

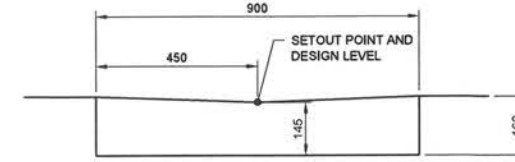
Scale 1:10

**SPECIAL OPEN CONCRETE INVERT 01 (SOCI-01)**

Scale 1:10

**SPECIAL MODIFIED OPEN CONCRETE INVERT 02 (SMOCI-02)**

Scale 1:10

**SPECIAL MODIFIED OPEN CONCRETE INVERT 03 (SMOCI-03)**

Scale 1:10



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CONSULTANT

AECOM Australia Pty Ltd  
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www.aecom.com**PROJECT**PARKES SECTION 3  
BLOCK 6**CLIENT**LAND DEVELOPMENT AGENCY  
Level 6 TransACT House  
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02 62073001 tel 02 6207 6110 fax  
www.lida.act.gov.au  
A.B.N 20 419 925 579**REGISTRATION****NOT FOR CONSTRUCTION****PROJECT MANAGEMENT INITIALS**

-	JP	JP
DESIGNER	CHECKED	APPROVED

**PROJECT DATA**

DATUM	SURVEY
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**ISSUE/REVISION**

B	DATE	DESCRIPTION
B	15.05.15	NCA AND TAMS SUBMISSION
A	02.04.15	DRAFT FSP
I/R	DATE	DESCRIPTION

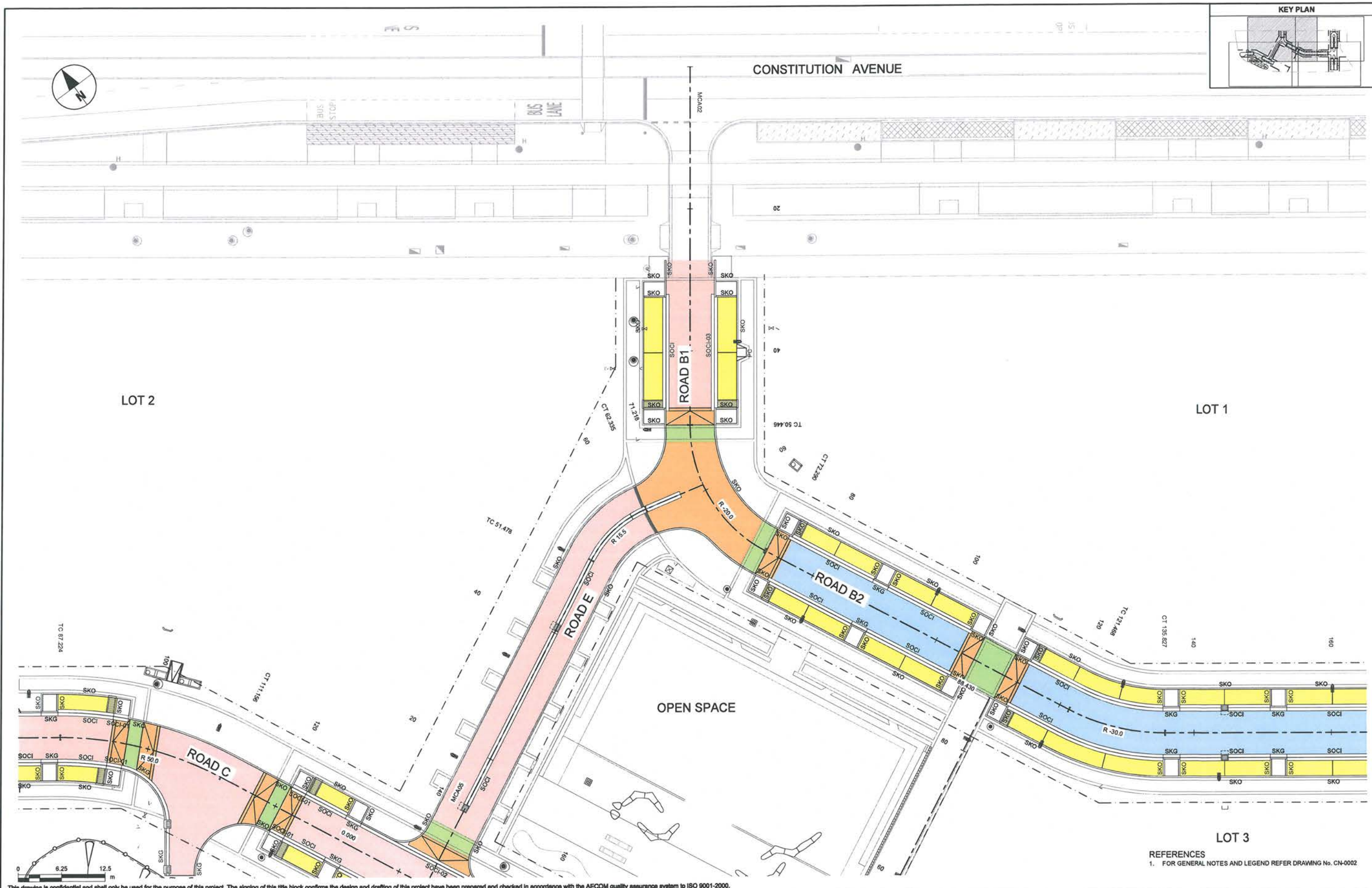
**PROJECT NUMBER**

60314240

**SHEET TITLE**PAVEMENT LEGEND  
AND KERB PROFILES**SHEET NUMBER**

60314240-SHT-20-NRTH-CP-1100




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PROJECT NUMBER

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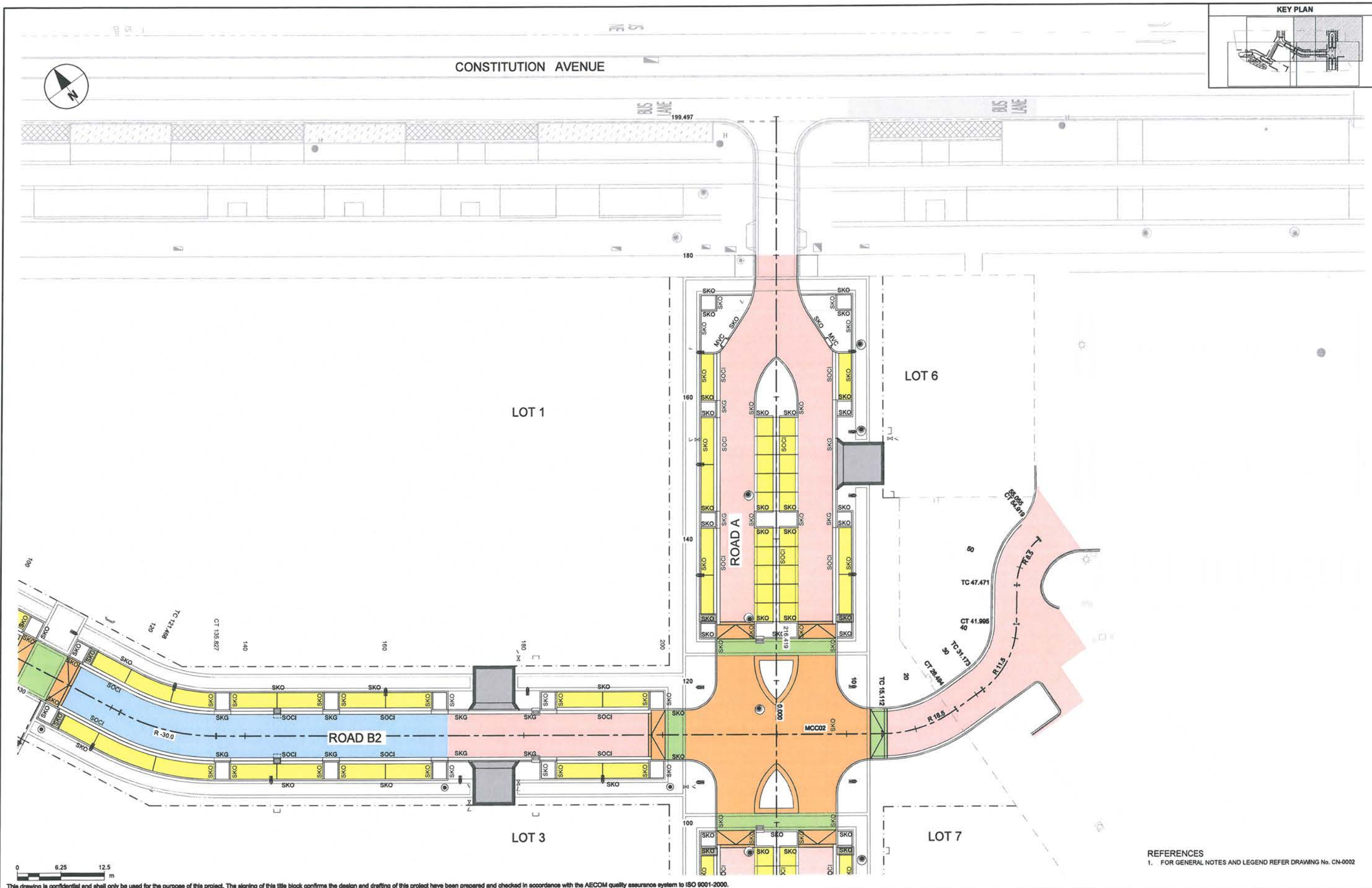
SHEET TITLE

 PAVEMENT PLAN  
 SHEET 1 OF 4

SHEET NUMBER

60314240-SHT-20-NRTH-CP-1101





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REFERENCES  
1. FOR GENERAL NOTES AND LEGEND REFER DRAWING No. CN-0002

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PROJECT NUMBER

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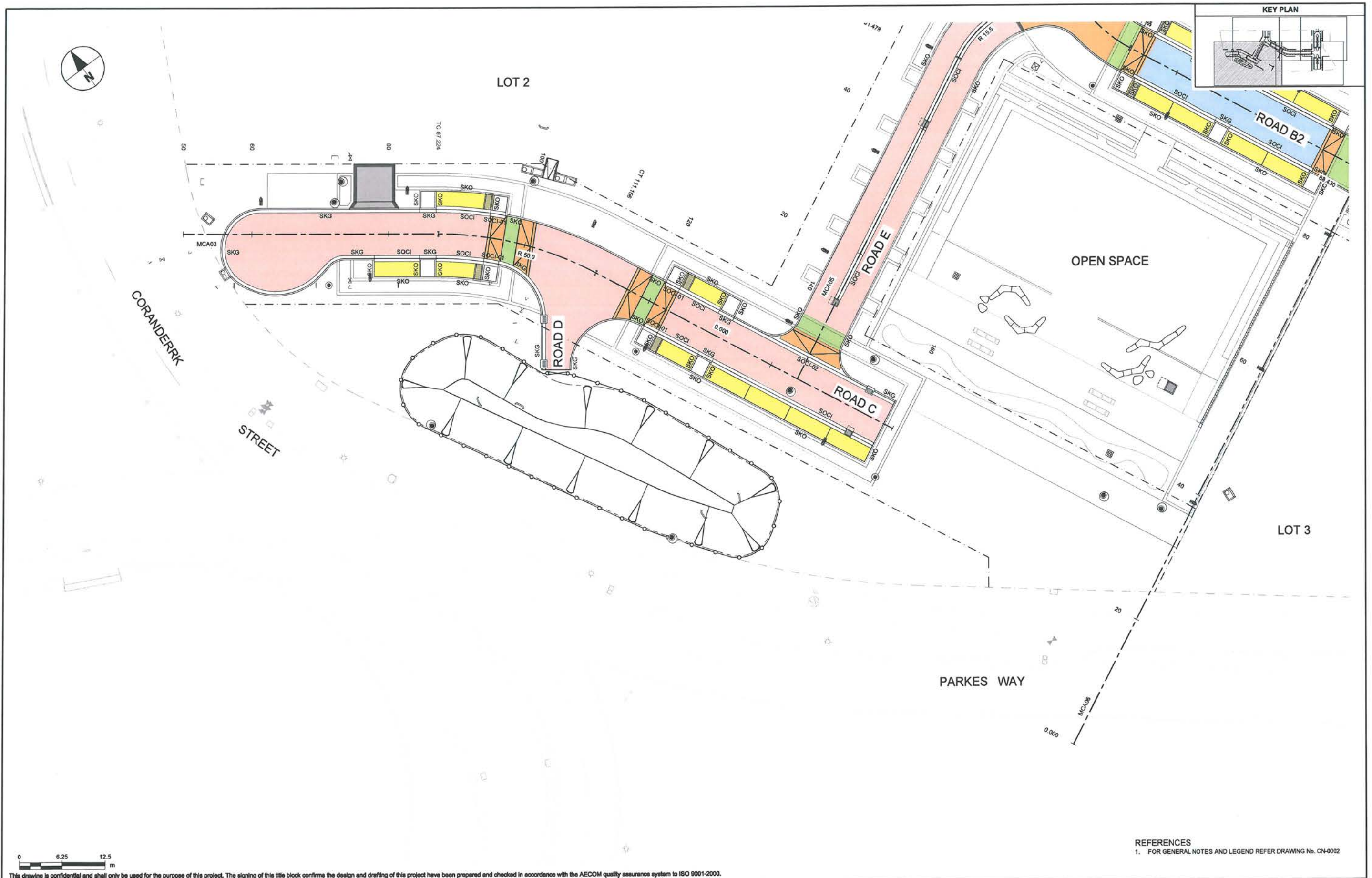
SHEET TITLE

PAVEMENT PLAN  
SHEET 2 OF 4

SHEET NUMBER

60314240-SHT-20-NRTH-CP-1102





0 6.25 12.5  
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PROJECT NUMBER

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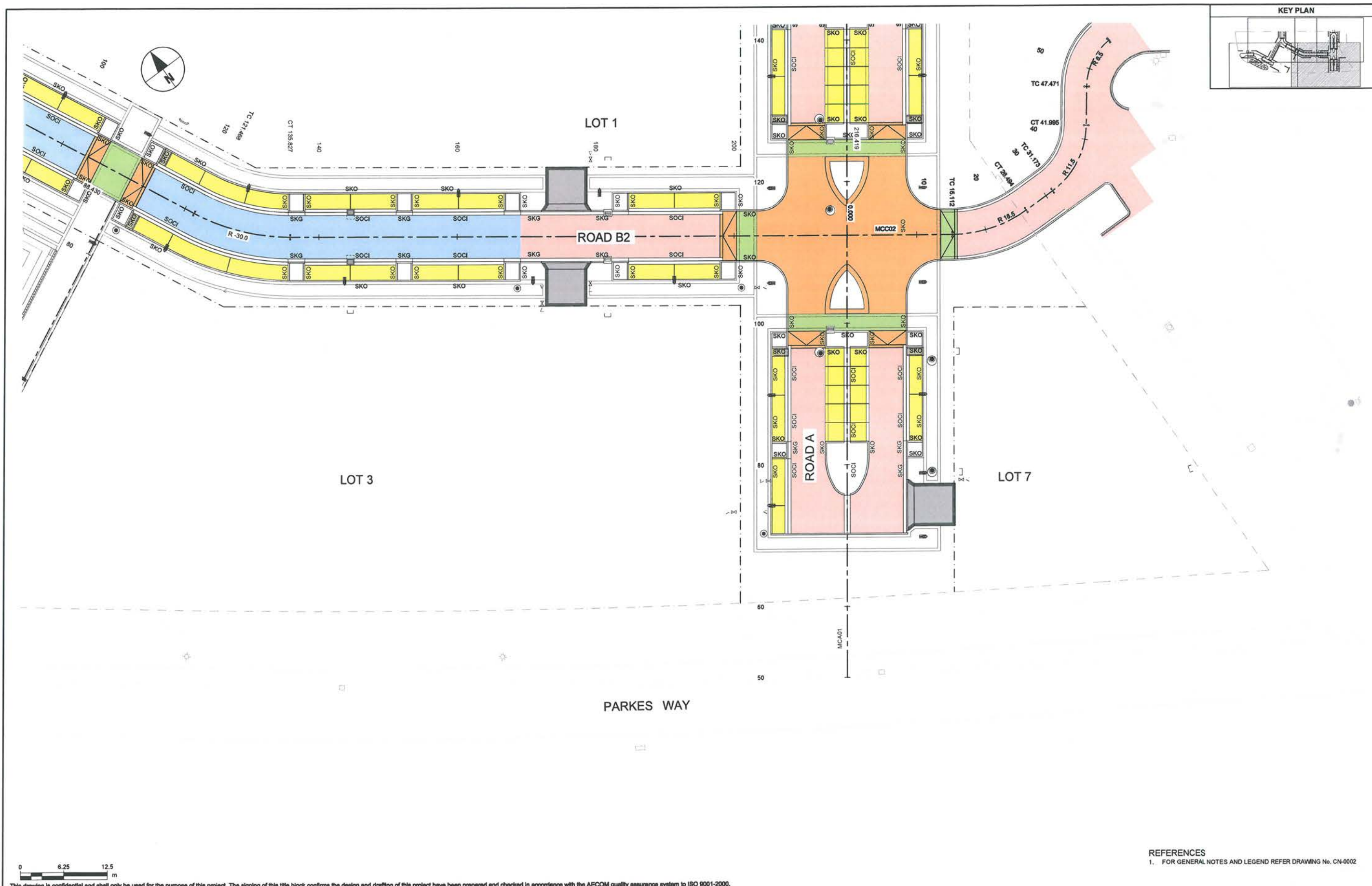
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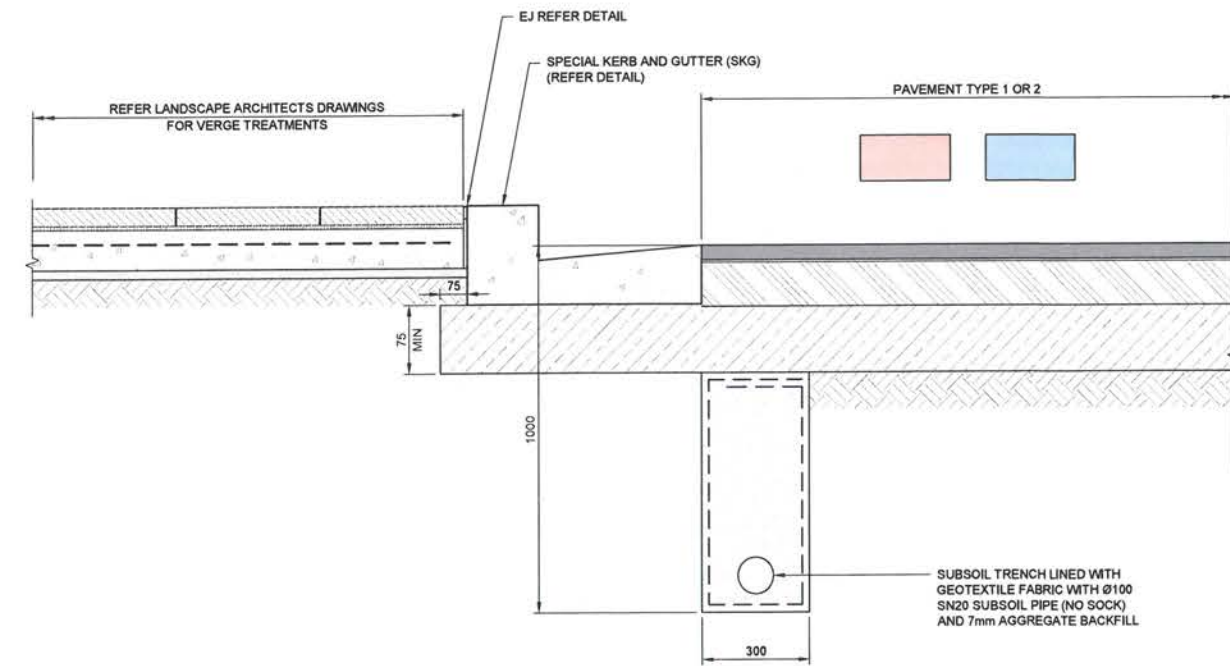
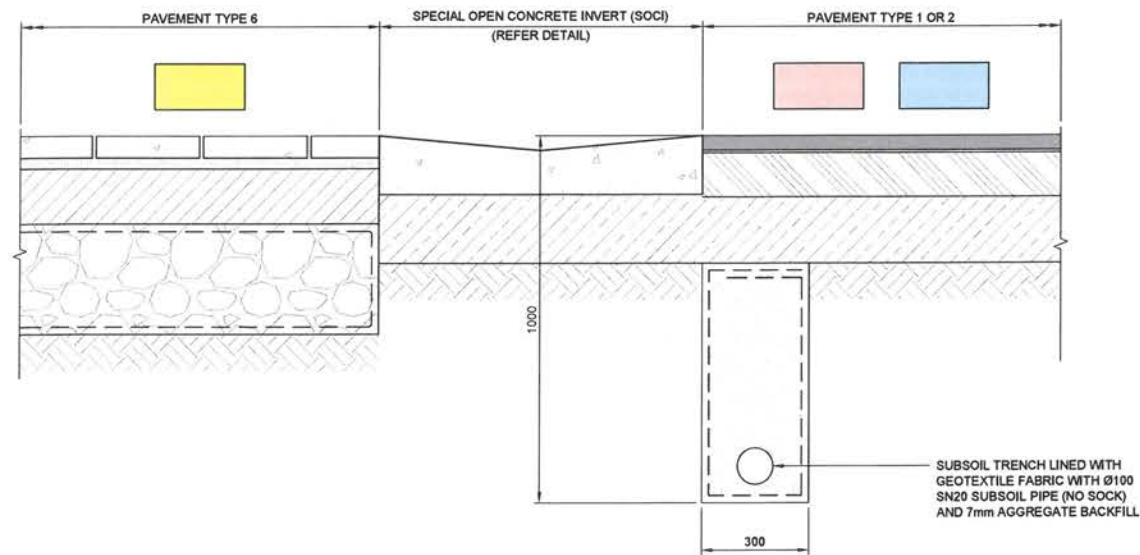
**PAVEMENT PLAN  
SHEET 3 OF 4**

SHEET NUMBER

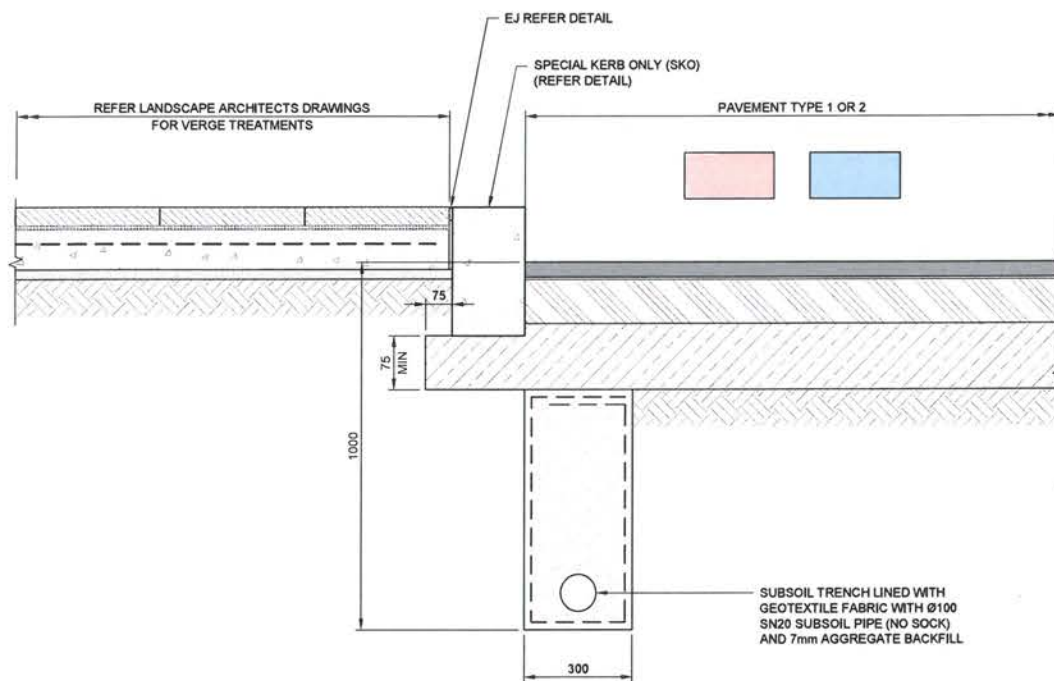
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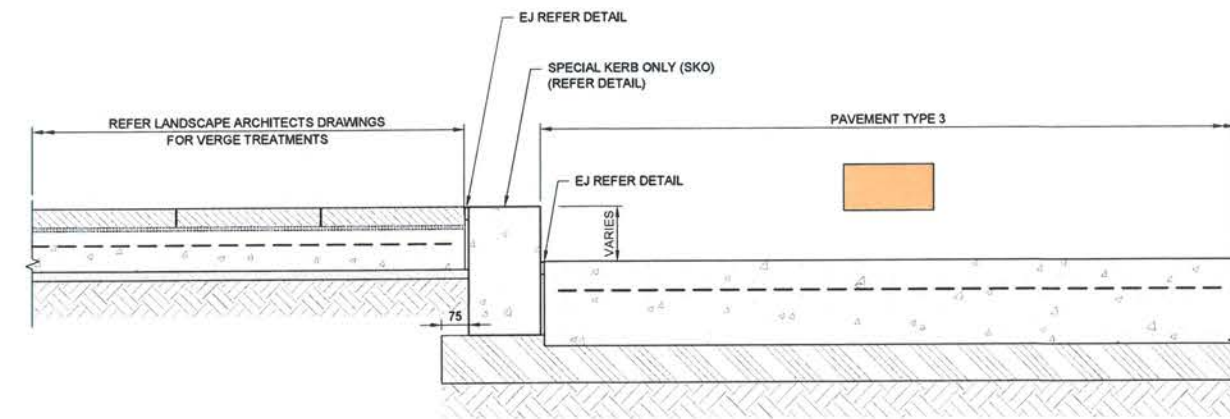
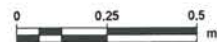




**EDGE - SPECIAL KERB AND GUTTER (SKG) ADJACENT TO PT1 AND PT2**  
NTS



**EDGE - SPECIAL KERB ONLY (SKO) ADJACENT TO PT1 AND PT2**  
NTS



**EDGE - SPECIAL KERB ONLY (SKO) ADJACENT TO PT3**  
NTS

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PROJECT DATA

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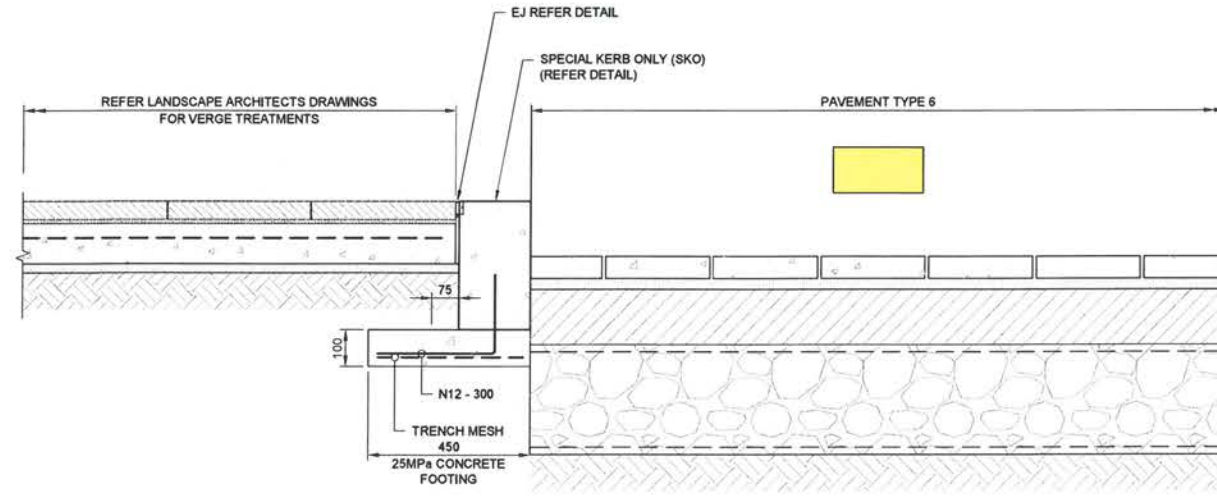
SHEET TITLE

PAVEMENT DETAILS  
SHEET 1 OF 4

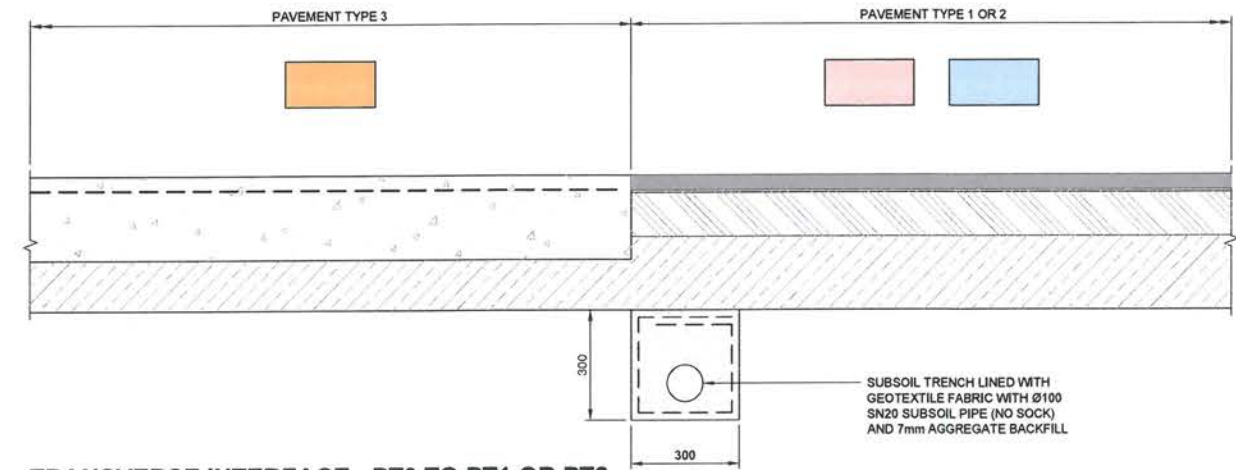
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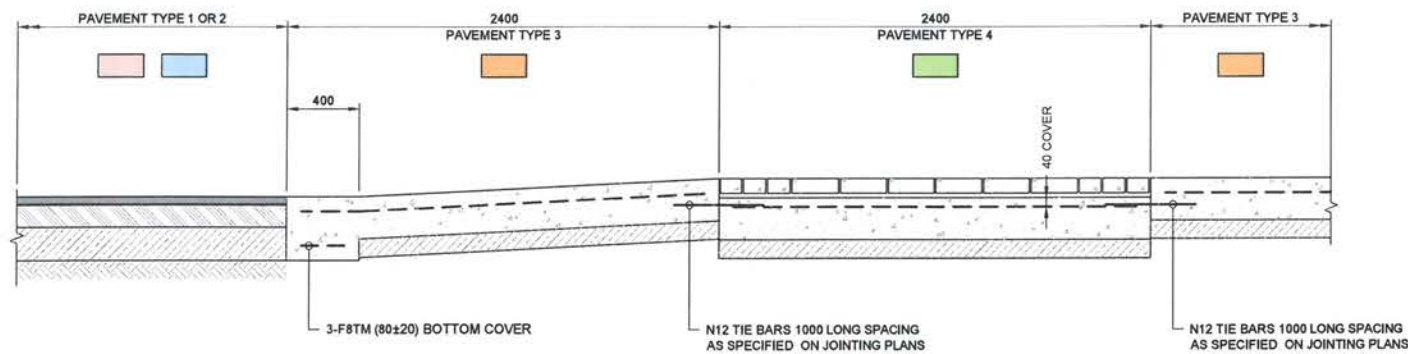


**EDGE - SPECIAL KERB ONLY (SKO) ADJACENT TO PT6**

NTS

**TRANSVERSE INTERFACE - PT3 TO PT1 OR PT2**

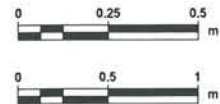
NTS

**TRANSVERSE INTERFACE - THRESHOLD**

SCALE 1:20

**NOTE**

1. THRESHOLD ON ROAD B2 SIMILAR



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I/R	DATE	DESCRIPTION

PROJECT NUMBER

60314240

SHEET TITLE

PAVEMENT DETAILS  
SHEET 2 OF 4

SHEET NUMBER

60314240-SHT-20-NRTH-CP-1112

PAVEMENT NOTES

REINFORCEMENT

R1 STEEL REINFORCEMENT MUST BE ACCORDANCE WITH AS4671 WITH THE FOLLOWING PROPERTIES:

TYPE	SHAPE	STRENGTH	DUCTILITY	NOTATION
BAR	DEFORMED	500 MPa	N	N12, N16
MESH	ROUND OR DEFORMED	500 MPa	L	SL82

- R2 MAXIMUM OF 3 LAYERS OF MESH AT ANY ONE POINT, NEST MESH WHERE POSSIBLE TO REDUCE LAP THICKNESS.
- R3 MESH REINFORCEMENT COVER TO TOP FACE, JOINTS AND EDGES 80 ± 20 MM. OUTSIDE BAR REINFORCEMENT COVER 80 ± 10 MM.
- R4 BAR BENDING DIMENSIONS ARE TO THE INTERSECTIONS OF STRAIGHT PORTIONS AT OUTSIDE OF BENDS. BENDING PIN DIAMETER TO BE SIX TIMES THE BAR DIAMETER.

KERBS

K1 KERBS AND MEDIANS MUST HAVE TRANSVERSE JOINTS AT LOCATIONS MATCHING THE J2 AND J8 JOINTS IN THE CONCRETE PAVEMENT BASE

CONCRETE BASE

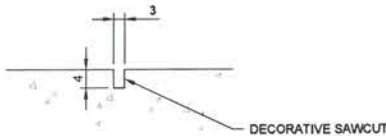
- P1 BEDDING AS PER PAVEMENT DETAILS.
- P2 REFER LANDSCAPE DETAILS FOR PAVER JOINTING AND CONSTRUCTION DETAILS
- P3 PROVIDE ISOLATION JOINTS IN PAVERS DIRECTLY ABOVE JOINT TYPES EJ AND J8 IN CONCRETE PAVEMENTS
- P4 PAVERS ADJACENT TO PAVER ISOLATION JOINTS MUST BE WHOLE PAVERS

CONCRETE BASE

- B1 CONCRETE BASE IS TO BE CONSTRUCTED IN ACCORDANCE WITH ACT STANDARD SPECIFICATION 05 EXCEPT THAT:
- CHARACTERISTIC COMPRESSIVE STRENGTH 40 MPa AT 28 DAYS
  - NO ADDITIONAL FLEXURAL STRENGTH REQUIREMENT
  - MAXIMUM AGGREGATE SIZE 20 mm
  - COLOUR AS NOTED ON DRAWINGS
- B2 THE SURFACE TEXTURE OF THE FINISHED BASE SHALL BE HESSIAN DRAG PARALLEL TO THE DIRECTION OF TRAFFIC, FOLLOWED BY BROOMING TRANSVERSE TO THE DIRECTION OF TRAFFIC. TEXTURING MUST COMMENCE AS SOON AS PRACTICABLE TO LIMIT SURFACE RAVELLING AND PRODUCE A UNIFORM FINISH WITHOUT ROUNDING OF THE PAVED EDGES. CURING MUST COMMENCE SOON AFTER TEXTURING IN ACCORDANCE WITH THE SPECIFICATION.
- B3 THICKNESS OF PAVEMENT TYPE 7 MUST MATCH THICKNESS OF ADJACENT CONCRETE AND SUB BASES, EXTEND FULL THICKNESS UNDER BOTH PAVEMENT TYPES.

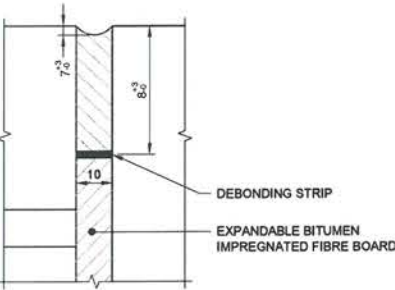
JOINT DETAILS NOTES

- IF THE CONCRETE PAVEMENT BASE IS CONSTRUCTED IN LONGITUDINAL SECTIONS (IE PARALLEL TO J2 JOINTS), EACH SECTION MUST TERMINATE AT A J7 JOINT TRANSVERSE TO J2 JOINTS, AND NOT AT A J8 JOINT. CONSTRUCTION JOINTS PARALLEL TO J2 JOINTS ARE NOT PERMITTED.
- A J7 JOINT, IF USED, MUST NOT BE LOCATED CLOSER THAN 1.5 M TO A J8 JOINT.
- THE NUMBERS OF TIE BARS ACROSS J2 JOINTS PER SLAB (IE BETWEEN ADJACENT J8 JOINTS OR BETWEEN A FREE END AND THE ADJACENT J8 JOINT) IS SHOWN ON THE DRAWINGS. THE TIE BARS CLOSEST TO THE ENDS OF THE SLAB MUST BE INSTALLED 350 TO 150 mm FROM THE END OF THE SLAB AND THE REMAINING BARS INSTALLED AT UNIFORM SPACING IN BETWEEN.
- RECESS CREATED BY TEAR-OFF CAPPED STRIP TO BE FILLED WITH SILICONE SEALANT TO MATCH CONCRETE (COLOUR TO BE APPROVED BY THE SUPERINTENDENT) UNDERLAID BY DEBONDING STRIP.
- KEY MUST BE CONCAVE IN FIRST CAST SLAB.
- THE FIRST CAST FACE MUST BE DEBONDED BY SPRAYING WITH A WAX EMULSION CURING COMPOUND WITHIN 10 DAYS PRIOR TO CASTING THE SECOND SLAB. THE WAX EMULSION CURING COMPOUND MUST COMPLY WITH AS3799 TYPE 2 CLASS A, APPLIED AT A RATE 25% GREATER THAN THE MANUFACTURER'S SPECIFIED RATE. THE COATING MUST BE INTACT AND EFFECTIVE AT THE TIME OF SUBSEQUENT CONCRETE PLACEMENT. STEEL TIE BARS MUST NOT BE SPRAYED.
- TOLERANCE ON KEY EDGE SLOPE 1 IN 10 TO 1.5 IN 10.

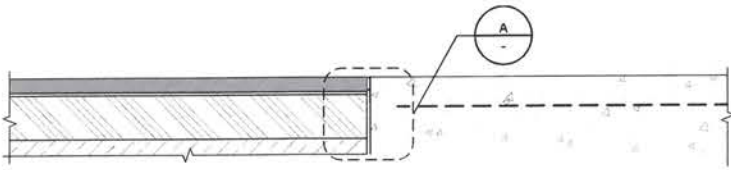


NOTE  
1. CONCRETE TO CURE FOR A MINIMUM OF 14 DAYS BEFORE SAW CUTTING

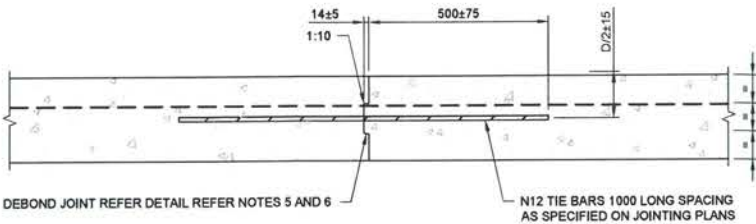
DECORATIVE SAWCUT  
SCALE 1:1



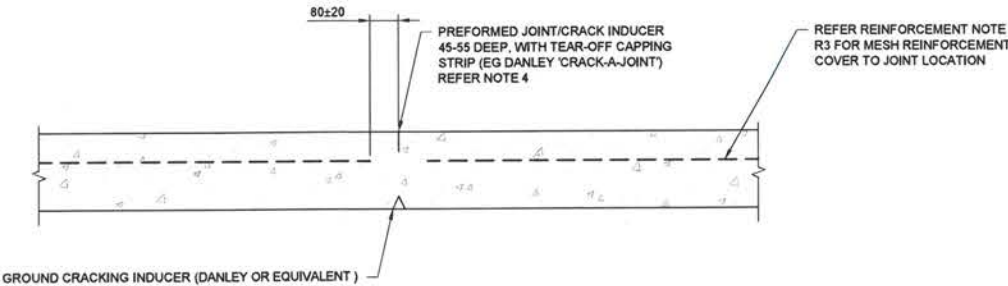
A EXPANSION JOINT  
Scale 1:1



EXPANSION JOINT TYPE EJ  
SCALE 1:10



CONCRETE JOINT TYPE 2 (J2) FORMED AND TIED JOINT  
SCALE 1:10



CONCRETE JOINT TYPE 8 (J8) INDUCED CONTRACTING JOINT  
SCALE 1:10



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REGISTRATION

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PROJECT MANAGEMENT INITIALS

DESIGNER	JP	JP
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PROJECT DATA

DATUM		SURVEY
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ISSUE/REVISION

ISSUE	DATE	DESCRIPTION
B	15.05.15	NCA AND TAMS SUBMISSION
A	02.04.15	DRAFT FSP

PROJECT NUMBER

60314240

SHEET TITLE

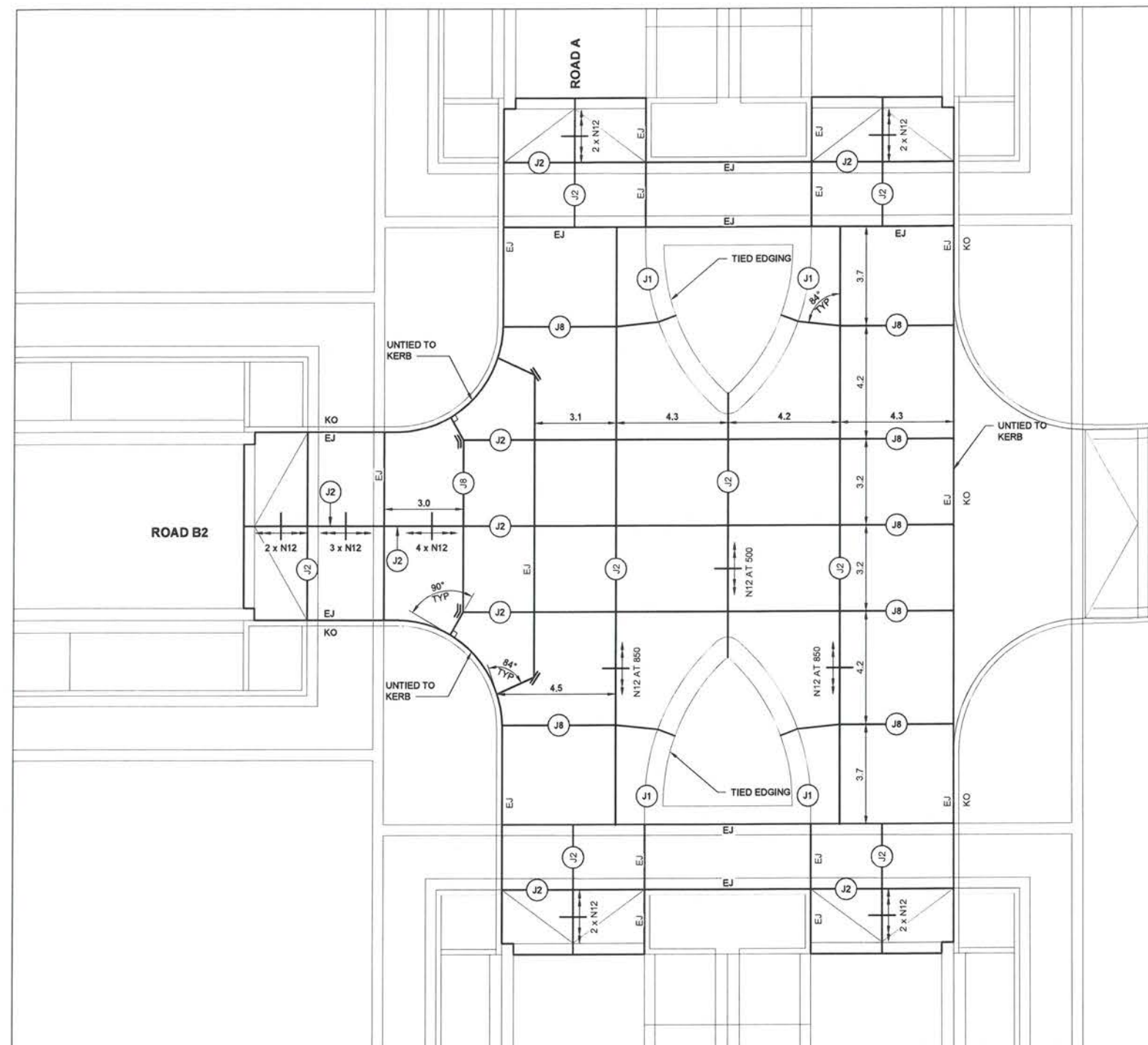
PAVEMENT DETAILS  
SHEET 3 OF 4

SHEET NUMBER

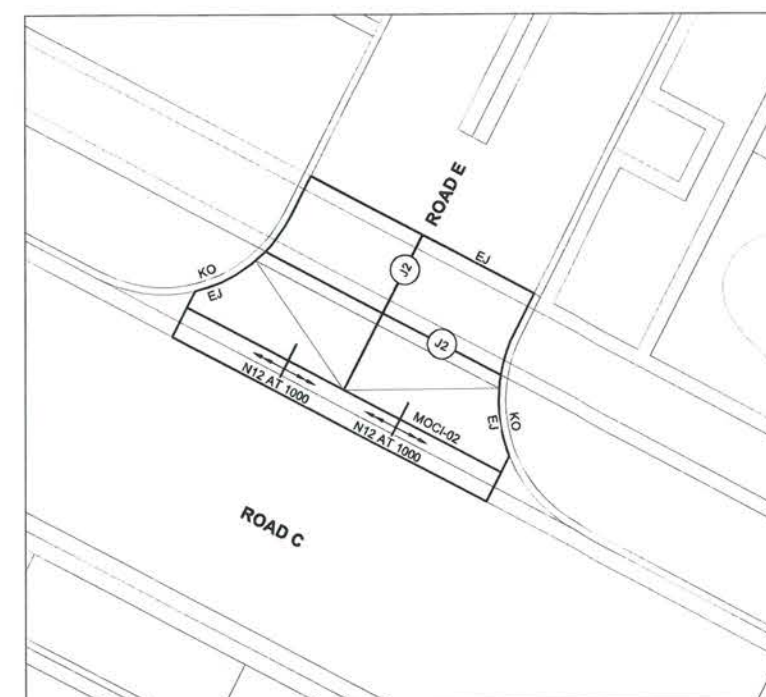
60314240-SHT-20-NRTH-CP-1113







**CONCRETE PAVEMENT JOINTING PLAN A (ROAD B2 AND A)**



**CONCRETE PAVEMENT JOINTING PLAN B (ROAD E AND C)**

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A	02.04.15	DRAFT FSP
I/R	DATE	DESCRIPTION

PROJECT NUMBER	60314240
SHEET TITLE	CONCRETE THRESHOLD JOINTING PLANS SHEET 2 OF 2
SHEET NUMBER	

60314240-SHT-20-NRTH-CP-1202